

Town of Branford, CT

ADA Self-Evaluation and Transition Plan for PROW Assets Volume 1



Main Street, Branford, CT

Volume 1: SETP Report
Volume 2: Appendix



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Table of Contents

Executive Summary	ES-1
Part 1 – ADA Self Evaluation	1
1.1 Introduction.....	1
1.1.1 <i>Self-Evaluation and Transition Plan (SETP) Purpose and Need.....</i>	1
1.1.2 <i>Mission.....</i>	1
1.2 Federal and State Accessibility Requirements	2
1.2.1 <i>Americans with Disabilities Act (ADA) of 1990</i>	2
1.2.2 <i>Rehabilitation Act of 1973 – Section 504.....</i>	2
1.2.3 <i>Adoption of Proposed Regulations/Guidelines - PROWAG</i>	3
1.2.4 <i>Connecticut Department of Transportation (CTDOT)</i>	3
1.3 Designation of Responsibility.....	4
1.3.1 <i>ADA Title II Coordinator</i>	4
1.3.2 <i>ADA Transition Plan Implementation Coordinator – Public Right-of-Way</i>	4
1.3.3 <i>Authority for Improvements.....</i>	4
1.3.4 <i>Oversight.....</i>	4
1.4 Non-Discriminatory Policies and Practices	5
1.4.1 <i>Public Notification.....</i>	5
1.4.2 <i>ADA Grievance Procedure</i>	5
1.4.3 <i>Public Engagement</i>	5
1.5 Proactive Measures	6
1.5.1 <i>Walkability Study for the Town of Branford (SCRCOG).....</i>	6
1.5.2 <i>Town Policies for Work Conducted Within the Public Right-of-Way</i>	6
1.5.3 <i>ADA Self-Evaluation and Transition Plan Efforts.....</i>	7
1.6 Methodology (Public Rights-of-Way)	7
1.6.1 <i>Outline.....</i>	7
1.6.2 <i>Assessment Approach</i>	7
1.6.3 <i>Pedestrian Network.....</i>	8
1.7 Data Collection and Inventory.....	9
1.7.1 <i>GIS Base Mapping</i>	9
1.7.2 <i>Sidewalk Data</i>	9
1.7.3 <i>Curb Ramp Data.....</i>	10
1.8 Assessment Results.....	11
1.8.1 <i>Initial Sidewalk Findings.....</i>	11
1.8.2 <i>Sidewalk Condition Assessment</i>	11
1.8.3 <i>Potential Sidewalk Gaps.....</i>	12
1.8.4 <i>Initial Curb Ramp Findings</i>	12
1.8.5 <i>Curb Ramp Compliance Assessment</i>	13
1.8.6 <i>Potential Missing Curb Ramps</i>	14

Part 2 - Section 504 Transition Plan.....	15
2.1 Proposed Improvements.....	15
2.2 Remediation Backlog	16
2.2.1 Sidewalk Backlog.....	16
2.2.2 Curb Ramp Backlog.....	17
2.2.3 Potentially Missing Ramps.....	18
2.3 Proposed Schedule of Improvements.....	19
2.4 Project Funding.....	22

Part 3 – Updates to the Transition Plan.....	23
3.1 Acknowledgement of ADA Transition Plan - PROW.....	23
3.2 Transition Plan Updates.....	24
3.2.1 Transition Plan Updates - Tracking Sidewalk Improvements.....	25
3.2.2 Transition Plan Updates – Tracking Curb Ramp Improvements	26
3.2.3 Tracking – Confirming Compliance	27
3.2.4 Technical Infeasibility Determination.....	27

Appendix (see Volume 2)

A. ADA Policy and Procedure Documents	A-1
B. Self-Evaluation – Project Prioritization.....	A-3
C. Self-Evaluation – Sidewalk Data.....	A-6
D. Self-Evaluation – Curb Ramp Data	A-49
E. Summary of Unit Costs.....	A-149
F. CTDOT ADA Technical Infeasibility Form	A-158

Executive Summary

BETA Group, Inc. (BETA) was selected by the Town of Branford (Town) to assist in complying with the Americans with Disabilities Act (ADA) by completing a Self-Evaluation and Transition Plan (SETP) for its pedestrian facilities within the public right-of-way.

Purpose and Need

The SETP is required for all municipalities under Title II of the ADA (Title II) and Section 504 of the Rehabilitation Act of 1973. Its purpose is to show efforts currently underway by the Town to comply with the terms of the ADA which prohibits discrimination based on a person's physical and mental abilities. While the Self Evaluation identifies existing barriers to accessibility to the Town's programs, services and activities, the Transition Plan serves as a guide for their removal.

Part One of this report is the **Self Evaluation (SE) portion of the SETP**. The SE confirms the Town's current status of the ADA's Title II requirements. Where they have not been fulfilled or are not currently available to the public, this report makes recommendations to meet or exceed the minimum requirements for compliance. These include:

- Designation of Responsibility - ADA Coordinator
- Public Posting of Non-Discrimination Policy
- Public Posting of Grievance Procedure
- ADA Self Evaluation and Section 504 Transition Plan

Additionally, this report serves as a planning tool for making necessary infrastructure improvements that will benefit the Town with a Geographic Information System (GIS) based inventory and condition assessment of its pedestrian network and will make recommendations that will align with the Town's overall asset management goals and objectives. At all points in this process, the Town will be in control and will reserve the right to provide reasonable accommodations as funding and other resources are available.

Methodology

BETA had conducted an inventory of sidewalks and curb ramps within the Town-maintained right-of-way during the development of the Branford Walkability Study conducted by the South-Central Council of Governments (SCRCOG) in 2023. The GIS-based inventory includes existing sidewalk polyline segments and existing curb ramps as point locations Town wide. Additional data including material type, dimensional measurements and condition data was collected and attributed to the entire inventory of sidewalk segments. Curb ramps were assessed for preliminary compliance against the Public Right of Way Accessibility Guidelines (PROWAG) standard and a specified subset of the overall inventory was further evaluated for condition and a more complete set of compliance criteria. Prior to compiling the data for

this report, BETA updated the 2023 inventory with data provide by Town staff which included improvements to existing sidewalk segments and curb ramps as well as new construction.

It is not within the scope of this study to understand the compliance history of individual elements of the network. This study includes an assessment of the existing conditions as of the date of this report and assumes that network elements were constructed to the standards in force at the time of construction.

The inventory and assessment of sidewalks and curb ramps, conducted in 2023 and updated in 2024 has identified obstacles to accessibility that will serve as a baseline for the ADA Transition Plan. Deficient items are identified based on current PROWAG standards. Where these elements do not meet the minimum threshold required, a recommendation is made for its removal and replacement to the requirements of the Architectural Access Board (AAB) using standard construction items and up to date unit costs based on Connecticut Department of Transportation (CTDOT) current bid pricing.

Improvements will prioritize those sidewalk and curb ramp elements that are located where pedestrian activity is expected to be the highest. To do this, a pedestrian demand model was developed based on an assessment that was conducted in which each inventoried item is assigned a priority level based on its location relative to heavily traveled corridors provided by the Town(see inset to the right).

PRIORITY LOCATIONS

- **Priority Corridors + 500 Feet**
 - East Main Street (Chestnut - N. Branford TL)
 - Florence Road (Alps - Jefferson)
 - Harbor Street (Maple - Cul De Sac)
 - Jefferson Road (W. Main - Burban)
 - Kirkham Street (Main-Maple)
 - Maple Street (Kirkham - Short Beach)
 - Main Street (N. Main - E. Main)
 - Montowese Street (Main St - S. Montowese)
 - Shore Drive (Short Beach - East Haven TL)
 - Short Beach Road (W. Main - Shore)
 - S. Main Street (Main - Montowese)
 - S. Montowese Street (Montwese - Sybil)
 - Sybil Avenue (S. Montowese - Limewood)
 - Thimble Island Road (School - Linden Point)
 - West Main Street (Gilbert - Main)

Findings

Overall, the inventory of sidewalks in 2024 was found to include approximately **34.8 miles** and the inventory of curb ramps was found to be **885**. BETA Team conducted field assessments in 2023 to confirm condition and accessibility of the Town’s sidewalks and curb ramp. **34.5 miles** of sidewalks were assessed for condition, and **445** curb ramps were assessed for condition and against the PROWAG standard. The remaining 440 curb ramp locations did not pass a preliminary PROWAG compliance assessment and have not been further inspected for condition.

An assessment has been completed by the BETA Team and is summarized in the report with details included in the Appendix (Vol. 2 Sections C and D). The reports tabulate observed surface material,

condition, and dimensional details that are relevant to current accessibility guidelines and identify any obstructions to continuous pedestrian access routes within the existing network.

The assessment summarizes the quantity of sidewalk and curb ramps that are to be replaced over time within each priority area, including “poor” condition sidewalks and non-compliant curb ramps. The resulting backlog is then translated into a dollar amount for the purposes of planning for and implementing the improvements.

It was found that 3.92 miles, or approximately 11% of the Town-wide network, was found to be in “poor” condition and have been recommended for replacement. An additional 7 miles has been estimated to need replacement based on spot repairs along fair and good condition sidewalks and has been included in the backlog for a total of **11.2 miles**. In addition, 447 curb ramps were inspected against the minimum criteria for PROWAG compliance. Of these ramps, 370 ramps have not met all of the compliance criteria, and 438 remain to be assessed to confirm condition. The total number of non-compliant curb ramps and potentially missing ramps that have been recommended for replacement is **865 ramps**. The low rate of compliance is typical for municipal pedestrian networks due to the age of the network and evolving federal standards.

Using standard construction items and unit costs provided by CTDOT the estimated cost to replace all poor condition sidewalks and non-compliant curb ramps within the Town-maintained right-of-way is approximately **\$16.3M**. This includes the estimated cost to replace sidewalks (\$6.8M) and non-compliant curb ramps (\$9.5M) in 2024 dollars.

Part Two of this report is the Transition Plan (TP) portion of the SETP. The TP describes how the Town will approach remediating the deficiencies identified in the SE including funding and prioritizing improvement projects. The Town agrees to address the deficient items in the highest priority categories wherever practicable and as funding allows.

Implementation

Prior to final approval of this Transition Plan, the Town will confirm the anticipated annual budget that will fund the Transition Plan and will serve as a guide for planning accessibility improvements to its public right-of-way. Tables ES-1 and ES-2 represent a range of possible scenarios for a Transition Plan approach.

The Town proposes a Transition Plan based on an **annual budget of \$185,000** as shown in Tables ES-1. This amount is based on available general funding from the Town (Engineering and DPW combined) on an annual basis. The balance of the compliance backlog will be addressed as additional discretionary funding is available in order to minimize the duration of the remediation timeline.

The Town intends to remediate sidewalk and curb ramp accessibility deficiencies, including poor-condition sidewalks and non-compliant curb ramps, in conjunction with roadway reconstruction projects and as standalone projects as funding allows. The Town recognizes that an annual budget of \$551,000 is

required for the Town to replace non-compliant curb ramps and poor-condition sidewalks over 30 years as shown in Table ES-2.

Proposed Annual Budget Scenario (2024\$)	Proposed Annual Budget - Sidewalks (2024\$)		Proposed Annual Budget - Curb Ramps (2024\$)		30 Year Cost/Benefit Combined	
	Miles of Sidewalk	Estimated Construction Cost	Number of Ramps	Estimated Construction Cost	Total Spending (2024 \$)	% of Overall Non-Compliant Backlog
\$ 185,000	0.10	\$ 65,000	11	\$ 120,000	\$ 5,550,000	34%

Table ES-1: The Town proposes an annual budget of \$185,000 (2024 \$\$\$) to remediate non-compliant curb ramps and poor condition sidewalks. Additional remediation will be conducted using discretionary funding sources when available.

Schedule of Remediation		Annual Budget	Spent	Total Remaining Backlog	% Remaining
Year	0	\$ 550,733	0	\$ 16,522,000	100%
Year	5	\$ 550,733	2,753,667	\$ 13,768,333	83%
Year	10	\$ 550,733	5,507,333	\$ 11,014,667	67%
Year	25	\$ 550,733	13,768,333	\$ 2,753,667	17%
Year	30	\$ 550,733	16,522,000	0	0%

Table ES-2: Total annual budget required to remediate sidewalks and curb ramps within 30 years

This proposed schedule represents the Town’s good-faith effort to improve sidewalk conditions within its Public Right of Way where reasonably possible and when funding is available.

Part Three of this report is for documenting updates to the SETP document. This SETP document is meant to serve as a guide for planning full compliance of the Town’s sidewalks and curb ramps within a time frame determined by the Town and based on anticipated funding and on-going related initiatives within the Town. It is intended to be a living document that is updated once improvements are made and remain available for public reference. It represents the Town’s on-going efforts to comply with the requirements of the ADA and its overall commitment to achieve accessibility to its pedestrian facilities for all its citizens.

Part 1 – ADA Self Evaluation

1.1 Introduction

1.1.1 *Self-Evaluation and Transition Plan (SETP) Purpose and Need*

The Americans with Disabilities Act (ADA) is a civil rights law prohibiting discrimination against individuals on the basis of disability. The ADA was enacted on July 26, 1990, and consists of five titles outlining protections in the following areas:

- Title I. Employment
- Title II. State and local government services
- Title III. Public accommodations
- Title IV. Telecommunications
- Title V. Miscellaneous Provisions

Title II of ADA pertains to the programs, activities and services provided by public entities stating that “no qualified individual with a disability shall, by reason of such disability, be excluded from participation in or be denied the benefits of the services, programs, or activities of a public entity, or be subjected to discrimination by any such entity.” Under Title II¹, all public entities are required to complete a self-evaluation and provide a transition plan in accordance with 28 CFR Part 150.

The Town of Branford (Town) is dedicated to its commitment to provide programs and services that make reasonable accommodations for qualified individuals with disabilities, in accordance with all applicable federal and state laws. This Self Evaluation and Transition Plan of the Town’s sidewalks and curb ramps will serve as an attachment to a comprehensive ADA Self Evaluation and Transition Plan and includes data and findings relative to pedestrian infrastructure within the public right-of-way.

With this Self-Evaluation and Transition Plan, the Town of Branford, as a municipal agency, has voluntarily entered into a revolving program to evaluate its policies, programs, and services to become into, and maintain, compliance with Title II of the Americans with Disabilities Act (Title II) and Section 504 of the Rehabilitation Act (Section 504).

1.1.2 *Mission*

The Town aspires to create and preserve a comprehensive transportation network that provides safe and accessible options for all modes of travel and for people of all abilities. The Town is committed to accessible principles in policy and practice and has taken multiple steps to create workflows, shape internal policies, and allocate funding to transform its transportation infrastructure into an accessible network.

¹ Department of Justice 28 CFR Part 35

BETA Group, Inc. (BETA) has prepared this report on behalf of the Town of Branford to provide Geographic Information System (GIS) mapping and ADA compliance services to develop an inventory and condition assessment of the Town’s publicly owned pedestrian infrastructure. The primary purpose of this project was to develop detailed data relative to these assets, in an effort to support the compliance measures and planning initiatives of the ADA and Section 504. These services were sought to enhance the Town’s capability to manage and track its data through a database and GIS environment. The completed evaluation assists the municipality with identifying, prioritizing, and removing its barriers to accessibility within the public right-of-way in a way that is fiscally manageable.

This report assumes it was the Town’s intent to meet all requirements and reasonable accommodations for public access based on the relevant standards at the time of construction. It is beyond the scope of this report to identify the date of construction of each segment of sidewalk and curb ramp location. This report should serve as a guide to assess the current status and to plan for future improvements based on the current ADA requirements at the time of this report.

1.2 Federal and State Accessibility Requirements

This report includes an assessment of pedestrian facilities, including sidewalks and curb ramps, located within the public right-of-way as specified by the Town. While not a comprehensive assessment of all Town-owned public assets under the jurisdiction of the Title II mandate, this report will serve as a good faith effort on the part of the Town to provide improved accessibility to pedestrian facilities within the public right of way throughout the Town.

The following regulations apply within the Public Right-of-Way in the Town of Branford:

1.2.1 Americans with Disabilities Act (ADA) of 1990

The ADA ensures that disabled individuals shall not be excluded from participation in, or be denied benefit of, or be subject to discrimination under any public program or activity. The ADA that affects the operations of the Town include:

- Title II: Public Services
- Title III: Public Accommodations
- Title V: Miscellaneous Provisions (i.e., Construction Standards and Practices)

The ADA requires that all new facilities built by public entities must be accessible to and usable by people with disabilities. The United States Access Board (USAB), along with the Department of Justice (DOJ) and the US Department of Transportation (USDOT) sets out the minimum accessibility requirements for newly constructed facilities and alterations to existing facilities which have been updated over subsequent years.

1.2.2 Rehabilitation Act of 1973 – Section 504

Section 504 of the Rehabilitation Act of 1973 ensures that no otherwise qualified individuals with a disability in the United States shall, solely by reason of his or her disability, be excluded from the

participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving Federal financial assistance.

By completing and adopting this Transition Plan, the Town may receive funding directly from Federal sources or via third party transactions through State agencies.

1.2.3 Adoption of Proposed Regulations/Guidelines - PROWAG

The U.S. Access Board (ABA) has approved accessibility guidelines for the design, construction, and alteration of pedestrian facilities in the public right-of-way, known as the Proposed Right-of-Way Accessibility Guidelines (PROWAG)² that shall be used by public agencies to ensure compliance with minimum accessible standards.

The PROWAG Standard has been considered Best Practices by the State of Connecticut since 2019 and has been the standard for new construction within the public right-of-way. In 2024, the DOJ and USDOT formalized PROWAG as the minimum standard for pedestrian facilities within the public right-of-way nationwide.

1.2.4 Connecticut Department of Transportation (CTDOT)

The Connecticut Department of Transportation (CTDOT) maintains its own Construction Standards and Policies as outlined in the CTDOT Transition Plan for the Public Rights of Way (2019)³. CTDOT requires improvements implemented by the State or funded by the State to comply the PROWAG standards.

² <https://www.access-board.gov/prowag/>

³ <https://portal.ct.gov/dot/-/media/dot/documents/ddbe/ctdot-ada-transition-plan-092019.pdf>

1.3 Designation of Responsibility

1.3.1 ADA Title II Coordinator

The ADA Coordinator is responsible for coordinating the Town’s efforts to comply with the ADA and handles disability access issues. The Town of Branford has designated the following individual as ADA Title II Coordinator to fulfill these duties:

Margaret Luberda
Director of Human Resources
Town of Branford
1019 Main Street
Branford, CT 06405
(203) 315-0629
mluberda@branford-ct.gov

1.3.2 ADA Transition Plan Implementation Coordinator – Public Right-of-Way

The ADA Transition Plan Implementation Coordinator monitors the Town’s progress towards ADA compliance within the Public Right of Way and manages review and updates of this document. The contact information for the referenced officials/departments:

John Hoefflerle
Town Engineer
Town of Branford
1019 Main Street
Branford, CT 06405
(203) 315-0606
jhoefflerle@branford-ct.gov

1.3.3 Authority for Improvements

Per 216-36 of the Town’s Code of Ordinance, the Board of Selectmen “shall determine where and when sidewalks shall be installed on existing Town roads”.

Per 216-37 of the Town’s Code of Ordinance, the Town Engineer “shall designate the required specifications for sidewalks, shall superintend all original construction and repairs and shall act as Sidewalk Inspector and Enforcement Officer”.

Per 216-38 of the Town’s Code of Ordinance, the Town “shall pay the entire cost of original installation of sidewalks from an annual capital improvement appropriation to be requested in the annual budget.

1.3.4 Oversight

The Town does not have a Commission for Persons with Disabilities that serves as an advisor to the Town on issues relating to citizens with disabilities.

1.4 Non-Discriminatory Policies and Practices

The Town is required to adopt the following policies and practices and make public its ADA obligation. It is a best practice to make this information available, as well as to recognize ongoing efforts to provide access to public programs and services, with a separate window on the Town's website. This on-line window shall itself be accessible and should provide links to resources relating to the ADA and respective contact information. Within this site, users should be able to access the Town's Notice of Non-Discrimination and ADA Grievance Procedure, which are elaborated below⁴.

1.4.1 Public Notification

A Notice of Non-Discrimination that describes the municipal role in ensuring public access to Town programs, services, and policies is to be posted clearly as is required by Title 2 of the ADA. The Town has included notices of non-discrimination in job postings, various contract documents and other agreements. The Town is currently posting a non-discrimination notice⁵ on the Town website. A copy of this notice is provided in **Appendix A**.

1.4.2 ADA Grievance Procedure

A Grievance Procedure describes the municipal role in receiving, responding and addressing public complaints regarding barriers to accessing programs, services, and policies as is required by Title 2 of the ADA. The grievance procedure should identify the ADA Coordinator with direct contact information, a procedure for submitting a complaint, and the time frame for a response. The Town is currently posting the procedure to file a complaint⁵ on the Town website. A copy of this notice is provided in **Appendix A**.

1.4.3 Public Engagement

The ADA specifies that municipalities shall adopt, publicly post, and solicit input for their Self Evaluation and Transition Plan. The information should be accessible to the public, including people who have disabilities that affect communication, such as blindness, low vision, deafness, and hearing loss. Although no specific method to reach the public is specified by the ADA, notice should be provided over multiple media formats, such as the Town's website, print, radio, or television.

Furthermore, a public entity that employs 50 or more persons is required, for at least three (3) years following the completion of the Self-Evaluation and Transition Plan, to maintain the document on file and make available for public inspection:

- A list of the interested persons consulted
- A description of areas examined, and any problems identified
- A description of any modifications made

⁴ <https://archive.ada.gov/pcatoolkit/chap2toolkit.htm>

⁵ https://view.officeapps.live.com/op/view.aspx?src=https%3A%2F%2Fwww.branford-ct.gov%2Fsites%2Fdefault%2Ffiles%2Ffield%2Ffiles-docs%2Fexternal_policy_and_notice_of_non-discrimination1.docx&wdOrigin=BROWSELINK

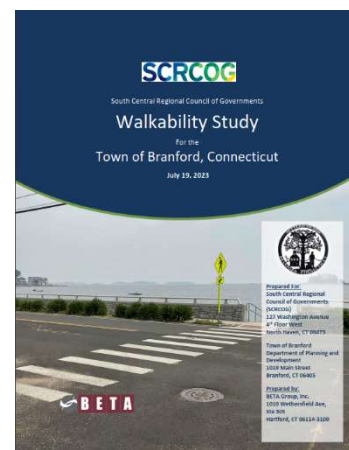
The Town intends to post a copy of this SETP on the Town's website and inform the public of the opportunity to review and comment on the Plan.

1.5 Proactive Measures

1.5.1 Walkability Study for the Town of Branford (SCRCOG)

The Branford Walkability Study (2023) made the following recommendations⁶:

- Adopt a Complete Streets Policy in Town
- Consult Connecticut Bicycle and Pedestrian Advisory Board (CBPAB) to assist the implementation of Town's walkability agenda
- Coordinate with CT Transit to review current locations of fixed route bus stops.
- Review existing enforcement of excessive vehicle speeds along priority corridors. Recommend increased patrols of high-hazard locations.
- Review details of State roadway ownership and maintenance agreement with the Town. Review policies related to sidewalks and related infrastructure.
- Use existing data to develop a formal ADA Self Evaluation and Transition Plan for the Town's pedestrian infrastructure. The SETP is a requirement for municipalities per Title 2 of the ADA and is consistent with the data collected in this report.
- Consider developing a wayfinding sign plan for priority corridors starting with Town Center.
- Study existing lighting along priority corridors to determine existing lighting is sufficient at the pedestrian scale. Look at key intersections where poor lighting poses a safety risk.



1.5.2 Town Policies for Work Conducted Within the Public Right-of-Way

The Town's Department of Public Works (DPW) has employed a workflow to tackle barriers to accessibility as outlined through this Plan. The DPW has also developed internal policies relating to construction within the right-of-way, such that all projects constructed by the Town shall be designed and constructed to meet federal regulations associated with pedestrian access. There are conditions which will be encountered; however, where full compliance is structurally impracticable, or technically infeasible, or where the cost of compliance is excessive without any substantial benefit to persons with disabilities, or prohibitive in some other manner. In these cases, variances from the rules and regulations are required (see Section 3.4.2).

⁶<https://www.dropbox.com/scl/fi/4tjbalo69ew36b9n9gete/Branford-Walkability-Plan-071923.pdf?rlkey=s7tp1s50nqbzs99wk2nrsjebg&e=2&dl=0>

1.5.3 ADA Self-Evaluation and Transition Plan Efforts

This plan represents a supplemental portion of an overall Self-Evaluation and Transition Plan required of municipalities under Title II of the ADA and Section 504 specifically related to the Town's sidewalks and curb ramps.

The Town has not developed a comprehensive ADA Self Evaluation and Transition Plan as is required by Title 2 of the ADA that addresses the many facets of public accessibility within its purview.

The development of this Plan is intended to assist the Town of Branford to be able to understand its pedestrian assets within the public right-of-way, and to systematically improve its transportation network. Improvements targeted through this plan focus on those which will benefit the mobility of the disabled to improve access for all road users alike. This Plan was developed to become the foundation for accessible design and construction practices within the public right-of-way in the Town of Branford.

Actions to implement compliance as required will solely be at the discretion of the Town and will be achieved based on available and allocated funding and feasibility of construction, among other challenges. It is the responsibility of the designers, engineers, and contractors that perform the work of such projects to ensure that the constructed end results meet all applicable accessible statutes and standards.

1.6 Methodology (Public Rights-of-Way)

1.6.1 Outline

Through the initial planning stages, meetings were held with the Town of Branford to align goals and set strategic milestones for the project. The overall outline for the development of the Self-Evaluation and Transition Plan follows the following key actions:

1. Identify and classify priority locations,
2. Conduct inventory and assessment including obstacles to accessibility,
3. Produce maps from existing conditions,
4. Analyze data and generate reports for planning purposes,
5. Discussion of findings and provide recommendations, and
6. Identify a proposed budget for removing obstacles to accessibility.

1.6.2 Assessment Approach

Creating a system that prioritizes key areas is vital for future planning and scheduling repairs. Therefore, to maximize efficiency, a tiered approach is utilized to classify specific locations that are prone to several variables. Variables may include:

- High pedestrian volumes
- Areas of known deficiencies or prior complaints

The Town has confirmed the following **Priority Tier** areas and buffer limits:

A. Priority Corridors + 500 feet

- East Main Street (Chestnut Street – North Branford Town Line)
- Florence Road (Alps Road – Jefferson Road)
- Harbor Street (Maple Street – Cul De Sac)
- Jefferson Road (West Main Street – Burban Drive)
- Kirkham Street (Main Street – Maple Street)
- Maple Street (Kirkham Street – Short Beach Road)
- Main Street (North Main Street – East Main Street)
- Montowese Street (Main Street – South Montowese Street)
- Shore Drive (Short Beach Road – East Haven Town Line)
- Short Beach Road (West Main Street – Shore Drive)
- S. Main Street (Main Street – Montowese Street)
- S. Montowese Street (Montowese Street – Sybil Avenue)
- Sybil Avenue (South Montowese Street – Limewood Avenue)
- Thimble Island Road (School Street – Linden Point Road)
- West Main Street (Gilbert Lane – Main Street)

B. Remaining Areas Outside of Priority Corridors

GIS analysis tools provide the capability to map these specific locations and buffer areas. The analysis presents an estimation of the likelihood of pedestrian usage and forms the basis of a heatmap, where a ranking can be assigned to each asset as a means to prioritize repairs. A map of the Town’s pedestrian priority areas can be found in **Appendix B**.

1.6.3 Pedestrian Network

Providing complete accessible pedestrian access within the public right-of-way is the goal of the Town of Branford. In addition to evaluating accessibility compliance, this self-evaluation identifies gaps in the existing pedestrian network. Most of the shortcomings in the network can be described as a barrier to accessibility, such as a physical obstruction or a structural defect. In creating a comprehensive network of accessible pedestrian routes, it is important to identify and plan to eliminate these obstacles. Some of these obstacles can include:

- Sidewalk and curb ramp surfaces that are not smooth, stable and meet the minimum criteria for PROWAG compliance,
- Crosswalk locations that lack compliant accessible curb ramps on one or both sides,
- Intersection corners where sidewalks lack curb ramps, and
- Missing sidewalks segments under 500’ in length or similar brief interruptions to a continuous sidewalk where there is no reasonable alternate route.

1.7 Data Collection and Inventory

1.7.1 GIS Base Mapping

Following industry standards, a program has been developed utilizing a GIS platform that includes a spatially located condition assessment of curb ramps and sidewalks. The database is structured to facilitate the development of an enhanced ADA Program for the Town.

GIS base mapping used for this Project includes the following data layers for reference:

- Existing Road Centerline
- Buildings
- Parcels
- Typical Pedestrian Generators (see 1.6.2)
- Natural features including waterways
- Connecticut DEEP Orthophotography

The availability of this base mapping allows the field inspection team to use a GIS-based data collection approach.

1.7.2 Sidewalk Data

Sidewalk data collection was conducted in the **Spring of 2023**. Sidewalk data collection utilized GIS-based linear sidewalk segments identified using desktop tools. Field crews inspected each segment of the Town's sidewalk network which included a total of approximately **35 miles** Town wide. Field crews inspected sidewalk segments confirmed material, surface condition and limited ADA compliance criteria.

Sidewalk data included the following attributes:

Defining Characteristics

- Sidewalk Location (Street Name, Street Segment, Odd/Even side)
- Material (Concrete, Bituminous, Other)
- Average Sidewalk Width (Feet)
- Length (Feet, Miles)

ADA Criteria Assessment

- Changes in Level (Lifting, Depressions, Tree Roots)
- Obstructions to a continuous pedestrian access route (48"-minimum clearance)
- Excessive cross-slope within the pedestrian access route (>2%)
- Surface Condition (Good, Fair, Poor)

1.7.3 Curb Ramp Data

The curb ramp data collection program was also completed during the **Spring of 2023**. BETA staff inventoried the Town's curb ramps along 100% of the sidewalk network described above using high-resolution aerial imagery (Nearmap 2023). A GIS-based point feature was created at each location for the purposes of collecting attribute data at each curb ramp location. A total of **885 ramps** were located.

In addition to location, the high-resolution imagery allowed BETA staff to identify preliminary ADA compliance criteria including surface material and the presence of a detectable warning panel. Of the 885 existing ramp locations, a total of **445 (50%)** passed this preliminary compliance assessment. The remaining 440 ramps were assumed non-compliant based on the lack of a detectable warning panel at the ramp opening as observed from aerial imagery.

BETA field crews inspected the specified **445 curb ramps** throughout Town. In addition to confirming the proper placement of the detectable warning panel, the physical inspection allowed for the condition and dimensions for each curb ramp to be established. Dimensional data for the 445 curb ramps was collected using a 2-foot smart level and tape measure per ADA guidance. Date-stamped pictures are also compiled and linked into the database, allowing each curb ramp to be viewed digitally.

The attributes assigned/collected for each curb ramp include:

Defining Characteristics/ Preliminary Compliance

- Curb Ramp ID (#)
- Curb Ramp Location (Street Name, Intersecting Streets)
- Material (Concrete, Bituminous, Other)
- Detectable Warning Panel Exists (Yes/No)

Condition Assessment

- Curb Ramp Condition (Good, Fair, Poor)

ADA Assessment

- Ramp Dimensions (Opening Width, Landing Width, Landing Depth) (inches)
- Landing Dimensions (Width, Depth) (inches)
- Ramp Slope Measurements (% grade)
- Landing Slope Measurements (% grade)
- Visible Obstructions (Catch Basin, Curbing, Hydrant, etc.)
- Marked Crosswalk (Yes/No)

1.8 Assessment Results

1.8.1 Initial Sidewalk Findings

Based on a review of the inventory data, a baseline of **34.79 miles** of sidewalk line features was created. Of the 34.79 miles, the majority was **concrete** (27.51 mi.) followed by **asphalt** (5.43 mi.), **brick** (1.79 mi), and **mixed materials** (0.07 mi). Sidewalk widths varied from 2 to 28 feet wide.

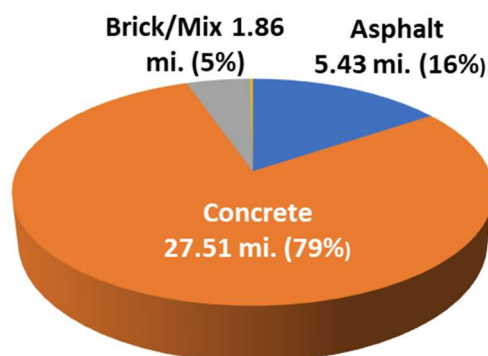


Figure 1.8.1: Town-wide assessment of sidewalk material

1.8.2 Sidewalk Condition Assessment

The majority of sidewalks in Branford or 18.11 mi. (52%) rated as **“Good”** condition, while 11.00 mi. (32%) rated as **“Fair”**, 3.92 mi. (11%) rated as **“Poor”**, and 1.76 mi. (5%) rated as **“New”** condition.

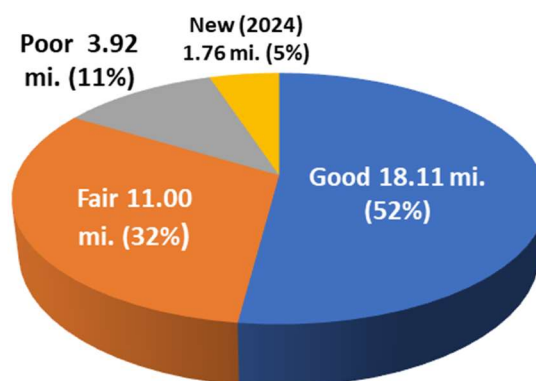


Figure 1.8.2: Town-wide assessment of sidewalk condition

For the purposes of the Transition Plan, the following recommendations have been assigned to each scoring category:

- Sidewalks considered to be in **“Poor”** condition displayed physical distresses affecting accessibility. **100% Replacement is recommended.**
- Sidewalks considered in **“Fair”** condition displayed some physical distresses. **Approximately 50% Replacement should be considered.**

- Sidewalks considered in **“Good”** condition displayed minor surface blemishes with no physical distresses. **Approximately 10% Replacement should be considered.**
- Sidewalks considered in **“New”** condition displayed no surface blemishes or physical distresses. **No Replacement should be considered.**

1.8.3 Potential Sidewalk Gaps

During field data collection, potential sidewalk gaps were identified. A sidewalk gap occurs where there is an interruption (<500’) in the sidewalk network that creates an obstacle to direct and convenient access to an existing accessible pedestrian route. Overall, a total of **12.27 miles** of sidewalk gaps were observed Town-wide. This data is for planning purposes only and should be evaluated on a case-by-case basis.

A detailed inventory of the Town’s sidewalks can be found in **Appendix C.**

1.8.4 Initial Curb Ramp Findings

Based on a review of the inventory data, a baseline of **885** features was created. Of the 885 curb ramps, **concrete** was the majority (779) followed by **asphalt** (60), and **brick** (46).

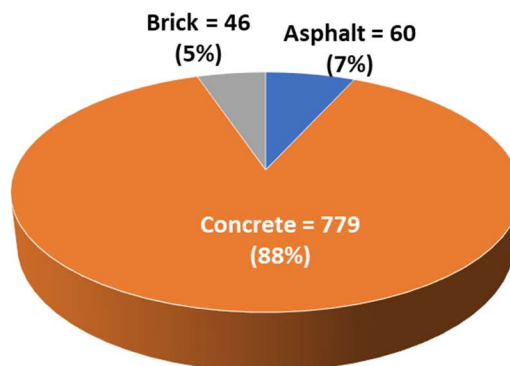


Figure 1.8.4: Town-wide assessment of curb-ramp material

Of the 445 ramps assessed for general surface condition, **210 (47% of ramps assessed)** were rated as **“Fair”** condition, **188 (42%)** were rated as **“Good”** condition, **44 (10%)** were rated as **“New”** condition, while **3 (1%)** were rated as **“Poor”**. 440 (50% of overall) existing ramps were not assessed for condition.

- Curb ramps considered in **“Good”** or **“New”** condition displayed no physical distresses.
- Curb ramps considered to be in **“Fair”** condition displayed some physical distresses; however, those distresses did not conflict with accessibility.
- Curb ramps that were classified as **“Poor”** displayed significant distresses that conflicted with accessibility thus resulting in a need for repair.

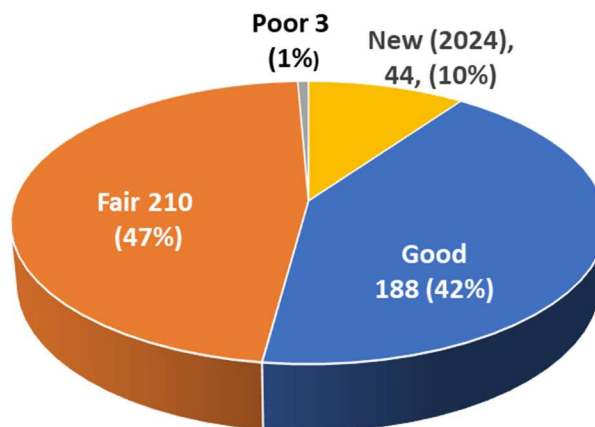


Figure 1.8.4a: Assessment of curb-ramp condition of 445 ramps assessed

A detailed inventory of the Town’s curb ramps can be found in **Appendix D**.

1.8.5 Curb Ramp Compliance Assessment

Curb ramps were assessed to the PROWAG standard. Under the PROWAG standard, each curb ramp was classified as either compliant or non-compliant. Of the **885** total ramps in Branford, **445 (50%)** total ramps passed the preliminary compliance assessment, which included surface material (concrete) and presence of a detectable warning panel (yes), while **440 (50%)** were determined to be non-compliant based on the limited set of criteria.

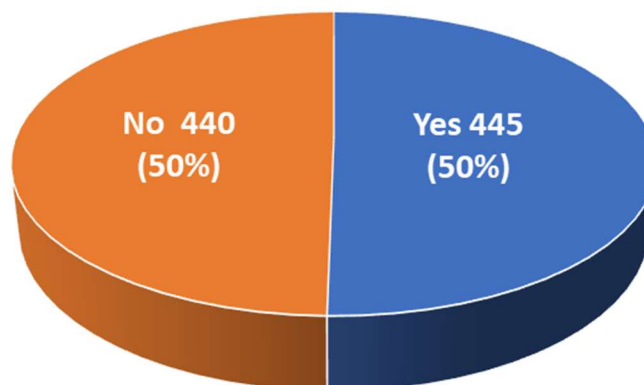


Figure 1.8.5: Preliminary assessment of curb-ramp compliance – 885 (100%) of overall ramp inventory

Ramps that passed the preliminary compliance assessment were further assessed by BETA field staff. A total of 445 ramp locations were field assessed for condition and compliance based on the following criteria:

- General surface condition,
- Properly installed detectable warning panel
- Dimensions (ramps, landings, transitions) (inches)
- Slope measurements (ramps, landings, transitions) (%)
- Visible obstructions (overgrown vegetation, catch basins, curbing, hydrants, etc.)

- Changes in level within the accessible route (> ¼")

Of the 445 ramps assessed for condition and PROWAG compliance, 77 (17%) ramps met the minimum criteria for accessibility. The remaining 368 ramps (83%) did not meet the minimum of at least one of the 30 assessment criteria. The low rate of compliance is typical for municipal pedestrian networks in the region due to the age of the network infrastructure and the evolving federal standards. It is not within the scope of this study to understand the compliance history of individual elements of the network. This study includes an assessment of the existing conditions as of the date of this report and assumes that network elements were constructed to the standards in force at the time of construction.

There are some instances where a non-compliant concrete ramp is a candidate for a “retrofit” or routine maintenance. This could be due to the following conditions:

- Missing or damaged detectable warning panel,
- Overgrown vegetation
- Correctable change in level at the ramp opening

In Branford there were a total of **74 ramps** that have been classified as potential retrofit candidates.

A detailed inventory of the Town’s curb ramp assessments can be found in **Appendix D**.

1.8.6 Potential Missing Curb Ramps

During field data collection, potential missing curb ramp locations were identified. A potential missing curb ramp occurs where there is a crosswalk with one or more missing curb ramps or where a sidewalk approaches a roadway but there is no apparent curb ramp to provide an accessible crossing of the road to an existing walkway on the opposite side. Overall, there are a total of **57 potential missing curb ramp locations** identified Town-wide. This data is for planning purposes only and should be evaluated on a case-by-case basis.

1.8.7 Curb Ramps Assessment Summary

	Total	Pre-Compliance	Town-Wide	Compliant	Non-Compliant
Total curb ramp inventory	885				
Ramps passing preliminary compliance assessment		445			
Town-Wide field assessment			445		
Compliance confirmed				77	
Non-compliance confirmed					808
Ramps not passing preliminary compliance assessment		440		TBD	
Total "potential missing" curb ramps for review	57			TBD	
TOTALS				77	808

Figure 1.8.5: Summary of curb ramp inventory and assessment (September 2024)

Part 2 - Section 504 Transition Plan

2.1 Proposed Improvements

Recommendations for the reconstruction and rehabilitation of sidewalks and curb ramps will follow current Town of Branford standards and specifications as well as the following:

- State of Connecticut Department of Transportation (CTDOT) Standard Sheets, Guides and Specifications for Highways and Bridges.

Existing sidewalks determined in need of replacement have been identified based on their surface condition as follows:

- Sidewalk segments identified as “New” = 0% replacement
- Sidewalk segments identified as “Good” = 10% replacement
- Sidewalk segments identified as “Fair” = 50% replacement.
- Sidewalk segments identified as “Poor” = 100% replacement

For the purposes of this planning schedule, the Town has indicated all new sidewalk construction would include replacement in-kind in regard to material and alignment.

Where an existing curb ramp does not meet the minimum dimensional requirements required by the current ADA Standard (PROWAG), a full replacement to the limits of and transition to existing sidewalk dimensions and grade is proposed. In the case of otherwise compliant ramps with a missing or damaged detectable warning panel, a “retrofit” is proposed which includes replacement of the ramp area only including a new detectable warning panel.

Unit costs for each treatment were the most recent bid pricing provided by CTDOT and are shown in Table 2.1.1.

Improvement Type	Unit Cost	Unit of Measure
Cement Concrete Sidewalk Reconstruction	\$ 760,000	/MI
Bituminuous Concrete Sidewalk Reconstruction	\$ 490,000	/Mi.
Curb Ramp Replacement or New Construction	\$ 12,000	/EA
Curb Ramp Retrofit	\$ 700	/EA

Table 2.1.1 Unit Cost Summary – Sidewalk/Curb Ramp Improvements⁷

⁷ Unit Cost Notes:

1. Estimate is for planning purposes only.
2. Unit Pricing based on CTDOT standard items and weighted average unit costs updated September 2024.

2.2 Remediation Backlog

2.2.1 Sidewalk Backlog

The proposed sidewalk repair and replacement backlog is based on the priority assessment described in Table 2.2.1. It is based on the condition assessment findings across the Town-wide network. The priority assignment for each sidewalk segment is based on condition (Good, Fair, Poor) and location (A or B) relative to high pedestrian activity as described in Section 1.6.2.

Condition/ Tier	Sidewalks - Asphalt	Sidewalks - Concrete/ Other Materials	Total Sidewalks	¹ Estimated Total Cost
Priority 1 - ("Poor" Condition) - Requires 100% Replacement				
1A	1.40	0.98	2.38	\$1,430,800
1B	1.12	0.42	1.54	\$868,000
Total	2.52	1.40	3.92	\$2,298,800
Priority 2 - ("Fair" Condition) - Requires 50% Replacement				
2A	0.65	6.95	7.60	\$2,800,250
2B	0.42	2.98	3.40	\$1,235,300
Total	1.07	9.93	11.00	\$4,035,550
Priority 3 ("Good" Condition) - Requires 10% Replacement				
3A	0.53	9.99	10.52	\$785,210
3B	1.27	6.32	7.59	\$542,550
Total	1.80	16.31	18.11	\$1,327,760
Priority 4 ("New" Condition) - Requires 0% Replacement				
4A	0.04	0.72	0.76	\$0
4B	0.00	1.00	1.00	\$0
Total	0.04	1.72	1.76	\$0
Total				
¹ Total	5.43	29.36	34.79	\$7,662,110

Notes:

¹ Sidewalk reconstruction is priced at \$490k per mile (asphalt) and \$760k per mile (concrete) based on a width of 5 feet. Pricing includes replacement in-kind and does not include curbing.

² Estimated construction costs are based on CTDOT standard items and weighted average unit costs updated September 2024.

Table 2.2.1 Transition Plan Summary – Sidewalk Improvements by Priority Phasing Strategy
(See Section 1.6.2 for Tier breakdown)

2.2.2 Curb Ramp Backlog

The proposed curb ramp repair and replacement schedule is based on the priority assessment described in Table 2.2.2. It is based on the condition assessment findings across the Town-wide network. The priority assignment for each non-compliant curb ramp location is based on condition (Good, Fair, Poor) and location (A or B) relative to high pedestrian activity as described in Section 1.6.2.

The repair backlog recommended for scheduled improvements includes all non-compliant curb ramps Town Wide (**808 existing locations**). Best practices suggest curb ramp repairs be coordinated with the Town’s roadway/sidewalk repair program.

Condition/ Tier	Existing Non-Compliant Curb Ramps - All Materials		Total Curb Ramp Backlog	¹ Estimated Total Cost
	³ Reconstruction Candidates	³ Retrofit Candidates		
Priority 1 - ("Poor" Condition)				
1A	268	0	268	\$3,216,000
1B	175	0	175	\$2,100,000
Total	443	0	443	\$5,316,000
Priority 2 ("Fair" Condition)				
2A	119	30	149	\$1,449,000
2B	53	5	58	\$639,500
Total	172	35	207	\$2,088,500
Priority 3 ("Good" Condition)				
3A	73	32	105	\$898,400
3B	46	7	53	\$556,900
Total	119	39	158	\$1,455,300
Total				
²Total	734	74	808	\$8,859,800

Notes:

1. Estimated construction costs are based on CTDOT standard items and weighted average unit costs updated September 2024.
2. Existing ramp reconstruction are estimated at \$12,000/location, Retrofit Candidates are estimated at \$700/location.
3. Retrofit Candidates are curb ramps that are concrete in "good" or "fair" condition that meet the minimum requirements for compliance with the exception of a missing or damaged detectable warning panel, or has a curbing or vegetation obstruction.

Table 2.2.2 Transition Plan Summary – Curb Ramp Improvements By Priority Phasing Strategy

(See Section 1.6.2 for Priority Tier breakdown)

2.2.3 Potentially Missing Ramps

The proposed new curb ramp placement schedule is based on the priority assessment described in Table 2.2.3. The priority assignment for each potentially missing ramp location is based on location (A or B) relative to high pedestrian activity as described in Section 1.6.2.

The new construction backlog recommended for scheduled improvements includes all potentially missing ramps Town-Wide (**57 locations**). Best practices suggest new curb ramp placement be coordinated with the Town’s roadway/sidewalk repair program.

Tier	Potentially Missing Curb Ramp	¹ Estimated Total Cost
Tier A - (Priority Corridors)		
A	22	\$264,000
Tier B - (Outside Priority Corridors)		
B	35	\$420,000
Total		
Total	57	\$684,000

Table 2.2.3 Transition Plan Summary – Potentially Missing Curb Ramp by Priority Phasing Strategy
(See Section 1.6.2 for Tier breakdown)

2.3 Proposed Schedule of Improvements

Prior to final approval of this Transition Plan, the Town will confirm the anticipated annual budget that will fund the Transition Plan and will serve as a guide for planning accessibility improvements to its public right-of-way. Tables 2.3.1 and 2.3.2 represent the proposed Transition Plan approach.

The Town has identified an annual budget of **\$185,000** for remediating existing non-complaint curb ramps and replacing poor condition sidewalks. Ramps in high priority locations and within the limits of roadway improvements that “alter”⁸ a pedestrian route will be prioritized from year to year. The Town recognizes that an annual budget of \$551,000, will remediate the non-compliant curb ramps and poor condition sidewalks identified in this report over a 30-year period. Improvements will be completed annually as funding allows. The Town recognizes that an additional annual balance of \$366,000 would be required to replace all non-compliant curb ramps and “poor” condition sidewalks over a 30-year period. See Table 2.3.1.

The Town has identified the following transition plan based on an annual budget of \$551,000 as shown in Table 2.3.2. The balance of the compliance backlog will be addressed as additional discretionary funding is available in order to minimize the duration of the remediation timeline. Table 2.3.2 shows the schedule of improvements required over 30 years with annual funding totaling \$551,000 including discretionary funds subject to availability.

Proposed Annual Budget Scenario (2024\$)	Proposed Annual Budget - Sidewalks (2024\$)		Proposed Annual Budget - Curb Ramps (2024\$)		30 Year Cost/Benefit Combined	
	Miles of Sidewalk	Estimated Construction Cost	Number of Ramps	Estimated Construction Cost	Total Spending (2024 \$)	% of Overall Non-Compliant Backlog
\$ 185,000	0.10	\$ 65,000	11	\$ 120,000	\$ 5,550,000	34%

Table 2.3.1: The Town proposes an annual budget of \$185,000 (2024 \$\$\$) to remediate non-compliant curb ramps and poor condition sidewalks. Additional remediation will be conducted using discretionary funding sources when available.

Schedule of Remediation		Annual Budget	Spent	Total Remaining Backlog	% Remaining
Year	0	\$ 550,733	0	\$ 16,522,000	100%
Year	5	\$ 550,733	2,753,667	\$ 13,768,333	83%
Year	10	\$ 550,733	5,507,333	\$ 11,014,667	67%
Year	25	\$ 550,733	13,768,333	\$ 2,753,667	17%
Year	30	\$ 550,733	16,522,000	0	0%

Table 2.3.2: Total annual budget required to remediate sidewalks and curb ramps within 30 years

This proposed schedule represents the Town’s good-faith effort to improve sidewalk conditions within its Public Right of Way where reasonably possible and when funding is available.

⁸ ADAAG Section 106.5: “[An Alteration is] a change to a building or facility that affects or could affect the usability of the building or facility or portion thereof. Alterations include, but are not limited to, ... resurfacing of circulation paths or vehicular ways, ... Normal maintenance, ...are not alterations unless they affect the usability of the building or facility.”

Baseline	Sidewalks to be Reconstructed/Added New (mi)	Annual Budget Required (2024 \$)	Estimated Total Construction Cost (2024 \$)
2024 Status	11.23	\$ 65,000	\$ 1,625,000
Program Year	Sidewalks to be Reconstructed/Added New Annually (mi)	Sidewalks Remaining Town Wide (mi)	Estimated Annual Budget Required ⁴
2024	0.10	11.14	\$ 65,000
2025	0.10	11.04	\$ 68,250
2026	0.10	10.95	\$ 71,663
2027	0.10	10.85	\$ 75,246
2028	0.10	10.75	\$ 79,008
2029	0.10	10.66	\$ 82,958
2030	0.10	10.56	\$ 87,106
2031	0.10	10.47	\$ 91,462
2032	0.10	10.37	\$ 96,035
2033	0.10	10.28	\$ 100,836
2034	0.10	10.18	\$ 105,878
2035	0.10	10.09	\$ 111,172
2036	0.10	9.99	\$ 116,731
2037	0.10	9.90	\$ 122,567
2038	0.10	9.80	\$ 128,696
2039	0.10	9.71	\$ 135,130
2040	0.10	9.61	\$ 141,887
2041	0.10	9.52	\$ 148,981
2042	0.10	9.42	\$ 156,430
2043	0.10	9.33	\$ 164,252
2044	0.10	9.23	\$ 172,464
2045	0.10	9.13	\$ 181,088
2046	0.10	9.04	\$ 190,142
2047	0.10	8.94	\$ 199,649
2048	0.10	8.85	\$ 209,631
2049	0.10	8.75	\$ 220,113
2050	0.10	8.66	\$ 231,119
2051	0.10	8.56	\$ 242,675
2052	0.10	8.47	\$ 254,808
2053	0.10	8.37	\$ 267,549
TOTAL	2.86	8.37	\$ 4,318,525

Notes:

1. Construction cost estimate is for planning purposes only
2. Estimate based on weighted average unit cost for standard CTDOT (2024) items required for 5'-wide sidewalk in-kind replacement/reconstruction = \$682,220/mile.
3. Annual budget values assume 5% annual growth of construction cost.

Table 2.3.3: Detailed budget scenario for remediation of sidewalks over 30 years using projected findings

Baseline	Non-Compliant Ramps to be Replaced/Added New (each)	Annual Budget Required (2024 \$)	Estimated Total Construction Cost (2024 \$)
2024 Status	808	\$ 120,000	\$ 3,600,000
Program Year	Non-Compliant Ramps to be Replaced Annually (each)	Non-Compliant Ramps Remaining (each)	Estimated Annual Budget Required ³
2024	11	797	\$ 120,616
2025	11	786	\$ 126,647
2026	11	775	\$ 132,979
2027	11	764	\$ 139,628
2028	11	753	\$ 146,610
2029	11	742	\$ 153,940
2030	11	731	\$ 161,637
2031	11	720	\$ 169,719
2032	11	709	\$ 178,205
2033	11	698	\$ 187,115
2034	11	687	\$ 196,471
2035	11	676	\$ 206,294
2036	11	665	\$ 216,609
2037	11	654	\$ 227,440
2038	11	643	\$ 238,812
2039	11	632	\$ 250,752
2040	11	621	\$ 263,290
2041	11	610	\$ 276,454
2042	11	599	\$ 290,277
2043	11	588	\$ 304,791
2044	11	577	\$ 320,030
2045	11	566	\$ 336,032
2046	11	555	\$ 352,834
2047	11	544	\$ 370,475
2048	11	533	\$ 388,999
2049	11	522	\$ 408,449
2050	11	511	\$ 428,871
2051	11	500	\$ 450,315
2052	11	489	\$ 472,831
2053	11	478	\$ 496,472
TOTAL	330	478	\$ 8,013,594

Notes:

1. Construction cost estimate is for planning purposes only
2. Estimate based on weighted average unit cost for standard CTDOT items required for curb ramp replacement/reconstruction = \$10,965/ea.
3. Annual budget values assume 5% annual growth of construction cost.

Table 2.3.4: Detailed budget scenario for curb ramp remediation over 30 years using projected findings

2.4 Project Funding

Funding for recommended improvements will be sourced from a combination of fixed and discretionary funding sources. The Town proposes funding sidewalk and curb ramp improvements annually according to the following schedule:

Sidewalks and Curb Ramps	
Funding Source	Potential Annual Funding (\$\$)
Local Funds	\$ 185,000
LOTICIP/LOCIP	TBD
Community Connectivity	TBD
Town Aid for Roads	TBD
State TIP, Utilities, etc.	TBD
Anticipated Total	\$ 185,000

Table 2.3.5: Funding sidewalk and curb ramp repairs will include a range of sources including fixed and discretionary, from local, state and federal agencies.

Part 3 – Updates to the Transition Plan

3.1 Acknowledgement of ADA Transition Plan

The above ADA Self Evaluation and Transition Plan for Public Right of Way (SETP -ROW) is intended as an attachment to the Town's overall SETP. The Department of Public Works / Engineering acknowledges the findings and recommendations of this SETP-ROW and is hereby acknowledged by the Town of Branford, effective 3/24/2025.

(Date)

Signed:

Margaret Lubarda 3/24/25
Ms. Margaret Lubarda
ADA Title II Coordinator
Town of Branford

John Hoeffler 3/21/2025
Mr. John Hoeffler
ADA Transition Plan Public Right-of-Way
Implementation Coordinator

James B. Cosgrove 3/24/2025
Authorizing Official Date
James B. Cosgrove
First Selectman

3.2 Transition Plan Updates

This SETP document is meant to serve as a guide for planning full compliance of the Town's sidewalks and curb ramps within a time frame determined by the Town and based on anticipated funding and on-going related initiatives within the Town. It is intended to be a living document that is updated once improvements are made and remain available for public reference. It represents the Town's on-going efforts to comply with the requirements of the ADA and its overall commitment to achieve accessibility of its pedestrian facilities for all its citizens.

Once the final draft of is approved, this Transition Plan may be updated as public input is received, improvements are made, or additional funding becomes available. All materials related to updates to this plan should be included in this section.

3.2.1 Transition Plan Updates - Tracking Sidewalk Improvements

Condition/ Tier (2024 Status)	Sidewalks - Asphalt	Sidewalks - Concrete/ Other Materials	Total Sidewalks	Total Sidewalk Improvements	Running Balance of Total Remaining	'Estimated Total Cost
Priority 1 - ("Poor" Condition) - Requires 100% Replacement						
1A	1.40	0.98	2.38		2.38	\$1,430,800
1B	1.12	0.42	1.54		1.54	\$868,000
Total	2.52	1.40	3.92	0.00	3.92	\$2,298,800
Priority 2 - ("Fair" Condition) - Requires 50% Replacement						
2A	0.65	6.95	7.60		7.60	\$2,800,250
2B	0.42	2.98	3.40		3.40	\$1,235,300
Total	1.07	9.93	11.00	0.00	11.00	\$4,035,550
Priority 3 ("Good" Condition) - Requires 10% Replacement						
3A	0.53	9.99	10.52		10.52	\$785,210
3B	1.27	6.32	7.59		7.59	\$542,550
Total	1.80	16.31	18.11	0.00	18.11	\$1,327,760
Priority 4 ("New" Condition) - Requires 0% Replacement						
3A	0.04	0.72	0.76		0.76	\$0
3B	0.00	1.00	1.00		1.00	\$0
Total	0.04	1.72	1.76	0.00	1.76	\$0
Total						
¹Total	5.43	29.36	34.79	0.00	34.79	\$7,662,110

Notes:

¹ Sidewalk reconstruction is priced at \$490k per mile (asphalt) and \$760k per mile (concrete) based on a width of 5 feet. Pricing includes replacement in-kind and does not include curbing.

² Estimated construction costs are based on CTDOT standard items and weighted average unit costs updated September 2024.

Table 3.2.1 Transition Plan Progress Tracking Table – Sidewalks

3.2.2 Transition Plan Updates – Tracking Curb Ramp Improvements

Condition/ Tier (2024)	Existing Non-Compliant Curb Ramps - All Materials	³ Retrofit Candidates - Concrete	Total Curb Ramp Backlog	Total Curb Ramp Improvements	Running Balance of Total Remaining	¹ Estimated Total Cost
Priority 1 - ("Poor" Condition)						
1A	268	0	268		268	\$3,216,000
1B	175	0	175		175	\$2,100,000
Total	443	0	443	0	443	\$5,316,000
Priority 2 ("Fair" Condition)						
2A	119	30	149		149	\$1,449,000
2B	53	5	58		58	\$639,500
Total	172	35	207	0	207	\$2,088,500
Priority 3 ("Good" Condition)						
3A	73	32	105		105	\$898,400
3B	46	7	53		53	\$556,900
Total	119	39	158	0	158	\$1,455,300
Total						
²Total	734	74	808	0	-	\$8,859,800

Notes:

1. Estimated construction costs are based on CTDOT standard items and weighted average unit costs updated September 2024.
2. Existing ramp reconstruction are estimated at \$12,000/location, Retrofit Candidates are estimated at \$700/location.
3. Retrofit Candidates are curb ramps that are concrete in "good" or "fair" condition that meet the minimum requirements for compliance with the exception of a missing or damaged detectable warning panel, or has a curbing or vegetation obstruction.

Table 3.2.2 Transition Plan Progress Tracking Table – Curb Ramps

3.2.3 Tracking – Confirming Compliance

As part of overall efforts to address non-compliant infrastructure the Town will make efforts to improve accessibility within both the Town's routine maintenance and major infrastructure projects. Efforts will also be made to track the overall progress in remediating ADA deficiencies identified in the Self Evaluation.

Where improvements are made, the GIS-based data collection tools deployed in this Self Evaluation may be used to update data at each location. As built assessment data may provide the basis for annual updates and progress tracking until the entire backlog identified in this report has been completed.

3.2.4 Technical Infeasibility Determination

There may arise circumstances where PROWAG criteria cannot be met due to various existing conditions encountered while altering an existing facility. It is recommended that the municipal ADA Implementation Coordinator and municipal staff maintain a record of particular circumstances where efforts have been made but have fallen short of meeting minimum thresholds defined by PROWAG. In these cases, reasonable accommodation shall be provided to the maximum extent feasible. Details of efforts made to meet PROWAG should be documented by the Town and kept on file. Appropriate justifications may include:

- Underlying Terrain
- Right-of-Way Availability
- Underground Structures
- Adjacent Developed Facilities
- Drainage
- Presence of a Notable Natural Feature
- Presence of a Notable Historic Feature

Alterations within State-owned right-of-way or applying State funding should utilize the Technical Infeasibility Waiver application which has been included in Appendix F.